

# AIRSHOWS

## LAUNCESTON

The Phoenix Flyers of Launceston Scale Fly In is an annual event held around March each year at our club located on the Panshanger property, approximately 35km south of Launceston Tasmania. Considering the weather leading up to Sunday 26th of March was horrible, one would be surprised that it was perfect on the day. The only interruption was a slow start due to some very low fog, although this allowed ample time for the pilots briefing, and the ever-important chinwagging sessions between friends. As luck had it, at around 10:00am the sky had opened up and pilots raced to be the first person in the air.

This event has always been very well supported with pilots and visitors coming from many clubs throughout Tasmania. These include NorthWest Aeromodelers, Phantom Flyers from Hobart, and Launceston Model Aero Club. It was excellent to see many new models and it is always a pleasure to see that some models are still being scratch built. It goes to show these techniques are surviving in the contemporary world of ARF. Many Phoenix Flyers club members and other aeromodelers from around the state still spend many hours in our colder months planning and producing a new scratch built model for the next scale day.

The range and scale detail on most ARF models today has improved considerably. The ARF category has also enabled many newcomers' and people from across the state to



**Phoenix President Ben Goode presents Michael Gibson picks up the award for Best WW11**



### Mix of fog & low cloud delayed the start

participate in the event and this year there was lots of stiff competition for the trophy. Across all categories there were multiple excellent aeroplanes on display, some of the best ARF and scratch built within Tasmania and this presented a tough job for our judges. Within the Scale Fly In, models and pilots are judged on different aspects. Mainly the presentation in the air, the basic scale features of the aircraft, and finish for both ARF and Scratch built models.

Michael Gibson of Phoenix Flyers of Launceston was very happy with all of his flights with the 50cc powered Lysander. Michael won the trophy for 'Best WW11 Aeroplane'. Dean Williams from Phantom Flyers in Hobart won the trophy for 'Best Post WW11 ARF' with an excellent model PC 21.

The focus of the day was certainly on Kevin Hay's 1/3rd scale Stearman. Providing a beautifully finished biplane



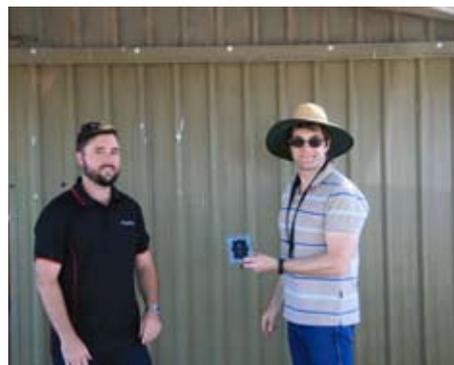
**Michael wheels out the Lysander. 118 inch wingspan, Powered by 50 cc PTE Engine with 22 x 8 prop, 9.6 kg**



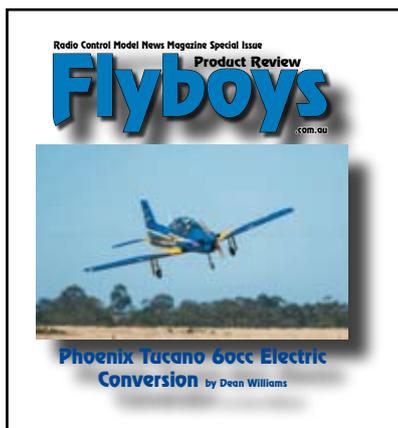
**Kevin Hay PT 17 1/3 scale Stearman, Wingspan 109 inch flying weight 24.8 kg, Engine Moki 215cc 5 Cylinder Radial**



**Best post WW12 scratchbuilt by Kevin Hay**



**Dean Williams from Hobart Phantom Flyers**



**Deans Williams 60cc electric conversion review in the previous issue, available separately at Flyboys \$2 shop**

of a past era, with stunning flights including lots of smoke! Kevin from Launceston Model Aero Club won the trophy for the 'Best Post WW11 Scratch Built'.

Andrew McEntyre was very excited to receive two awards for his 1913 82" wingspan Etrich Taube. Andrew had a number of flights with one flight being a little unusual to say the least. Providing some fancy freestyle flying after a landing approach being a little too fast. This received a loud applause



**Dean's Tucano won the award for Best Post WW11 ARF.**

from the spectators upon a successful recovery. This may explain winning the 'Pilots Choice Award'. However, Andrew also took home the trophy for 'Best Pre WW11 Aeroplane' with his lovely model. The Phoenix Flyers club President Ben Goode was on hand to present Trophies on the day.

In the end everyone had a fantastic day with excellent flying from all pilots without a single mishap, which is very pleasing. Congratulations to everyone who attended another great day of

flying at Phoenix Flyers. In 2018 we look forward to continuing the trend of good weather and company for the next Scale Fly In. On a final note, I would like to congratulate all the trophy winners and say thank you for the support I received to make these events the success they are.

Andrew McEntyre. Contest Director

**SCALE AEROBATICS VICTORIA**

Some five years ago IMAC, was quite big in Victoria, but over the past half

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**Andrew McEntyre 1913 Etrich Taube**  
**Wingspan 85" Flying weight 6.4kg, Engine Saito FG 20 4 stroke**  
**20cc petrol, with modified engine ignition kit from CH ignition**  
**USA, Covered in Solartex.**



**Andrew McEntyre winning Best**  
**Pre WW11 and Pilots Choice**  
**Award for his 1913 Etrich Taube**



**F8F Bearcat John Bowden from NWAM. F8F**  
**Bearcat, Cheng yang model, wingspan 96" weighs**  
**17kg's powered by 84cc Saito three cylinder**  
**radial, Rccskj power box with dual life batteries**  
**for controls and 6v for engine. Flight torque**  
**servos on control surfaces**



**Rodney Kreutzer Corsair.**  
**Wingspan 85", Flying weight 15.1kg, Engine**  
**Saito FG 84cc 3 cylinder Radial, Biela semi**  
**scale 22x10 3 blade propeller, Robart Electric**  
**Retracts, S-Bus servos with full telemetry**



**VALE Clive Butler.** *(Sad news of Clive's passing.*  
*Long may the memories of his excellent scale*  
*modelling pursuits serve to inspire others. ED)*



**Huw Davies P51 Mustang, powered by an OS**  
**2000 4 stroke engine, 2 metre wingspan at 6.8KG,**  
**Futaba T10CAG transmitter and receiver**

decade the number on pilots and events in Victoria have dropped off, to nearly zero.

I was appointed the Vic State Rep for the ASAA in November 2016 so as a part of this I was thinking of ways to get pilots flying IMAC again. I decided to run a Come and Try Day for IMAC at Northern Flying Group. The idea of this day was to introduce what IMAC (Scale Aerobatics) is all about. We would have experienced pilots available to help and will explain the current 2017 Basic IMAC sequence and fly it with the guidance of an experienced IMAC pilot.

There was no need for large IMAC planes. Any sports plane will do the Basic IMAC sequence. The weather was not looking good but someone must like me. Other than it being very cold the conditions for flying were very good.

We had some fourteen people turn up to the 9am start and started off with an introduction to scale aerobatics. This was a power point explaining the current IMAC Basic sequence and what the Judges look for. I wanted to make this part of the day as short as I could so we could get out and do some flying, but found the guys wanted to hear more and this ended up taking about forty five minutes.



**Stevie Malcman's Krill Extra 330. Da 120 Bielia 27x10**



**In terms of flying trim and ease of set up Extreme Flight are the class leaders in conventional balsa ply ARFs**



**Eager to learn**



**Welcome packs with schedules and stick plane**

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All that turned up received a welcome pack with the Basis Sequence, judging guide lines for the Basic sequence and a stick plane all on a clip board. After the intro, I went out and did a demo flight of the 2017 Basic sequence with all the pilots standing behind me. As I flew the sequence I explained a little about what I was trying to achieve. After this Demo flight, we then opened it up to all to give it a go. We started flying at 10am and finished around 4.30pm with a plane in the air all the time.

All in all the day was a success. Thanks to all who turned up to fly and help out. And a big thanks to the Northern Flying Group for letting us use the field, and Mark Sills for cooking lunch. I wanted to take more pictures but was way too busy helping all the pilots.

Michael Adrysik.

*(Pity I missed it. A job well done. You definitely don't need a big all composite model to have a go and join the fun. Ed)*



**A small hand held aeroplane is helpful to work through the manoeuvres, particularly the more difficult yet most enjoyable known and unknown schedules**

**Seymour IMAC**

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For more information:  
 Contact CD:  
 Michael Adrysik  
 +61413122035  
[mandrysi@bigpond.net.au](mailto:mandrysi@bigpond.net.au)

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**Working through the Hammerhead**

